

**REAR SUSPENSION COIL SPRING****To remove**

Jack up and put the car on stands.

Place a small hydraulic jack under the suspension, just behind the brake backing plate, in order to take the tension off the coil spring.

FIT THE CLAMP ROUND THE TRANSMISSION COUPLING and tighten. (See Section E for details of clamp.)

Mark the Rotoflex coupling and flanges before removing. It is important that the coupling is refitted in exactly the same position as before, or premature failure of the coupling may occur.

Remove the three bolts holding the drive shaft flange to the coupling and release the drive shaft.

Remove the road wheel.

Disconnect the handbrake cable from the backing plate.

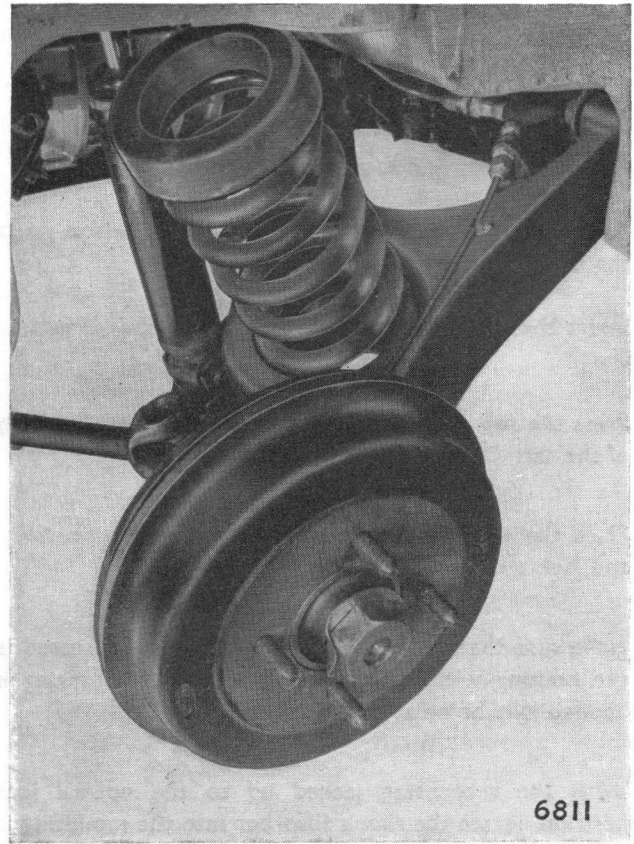
Remove the bolt retaining the shock absorber to the suspension arm.

Lower the jack until the suspension is fully extended and remove the jack.

The suspension will now hang on the two front pivot brackets, and the coil spring assembly can be removed.

Certain models are fitted with a rubber packing piece between the spring and the suspension arm.

Certain models are fitted with a longer spring.



**Fig. 2.** Coil spring removed from body

The method of checking which spring is fitted, is by removing the spring from the car, according to the instructions given in this Section, and measuring the free length of the spring. (See General Data.)

Should the car be fitted with short springs without packing, a long spring may be used as a replacement, PROVIDING a packing piece is fitted between the spring and the suspension arm on the other side.

If the car already has packing pieces fitted, then fit the longer spring on the one side, retaining the packing piece on the other side only.

When fitting a packing piece, ensure that it is fully seated in the suspension arm, and that the spring is located correctly in the packing piece.