

DESCRIPTION

The heater will deliver hot or cold fresh air to either the windscreen or the car interior or a proportion to both. The blower (if fitted) greatly increases the amount of air fed into the heater.

Hot water from the engine cooling system is fed into the heater. The supply is regulated by means of a cable actuated water valve mounted on the bulkhead; the control, "Car heat", being mounted on the fascia.

"Car heat" control.—This has two main positions: "Blue" (cold or off), and "Red" (hot); any intermediate position will give a control of heat according to the setting of the lever.

The supply hose is routed from the cylinder head via the right-hand rear wheel-arch and body sill ducting to the front wheel-arch and so to the water valve. The return hose is routed in the opposite direction via the left-hand sill ducting and rear wheel-arch to the radiator bottom tank or by-pass junction (if fitted).

As the heater is above the level of the cooling system it is necessary to bleed after filling. On early cars the bleed valve is fitted at the heater outlet pipe (see Fig. 5). On later models the bleed valve will be fitted into the engine cooling system.

Whenever bleeding is necessary, ALWAYS REFER TO SECTION "A", for the appropriate instructions.

The heater distribution control has four positions:—

- 1. "OFF" (uppermost).
 - 2. SCREEN.
 - 3. SCREEN and CAR.
 - 4. CAR (lowest).
- } See Fig. 1.



Fig. 2. Blower unit and air hose

BLOWER

To remove and refit (See Figs. 2 and 3)

Disconnect battery.

Disconnect air hose at the blower end. (Fig. 2.)

Remove grille panel.

Remove the four nuts with spring washers; these are located inside the plenum chamber (See Fig. 3), and withdraw the blower complete with filter gauze.

Release the cable from the harness clips, disconnect at blower switch and withdraw into the luggage compartment or at the connector tube near blower.

Refitting is a reversal of the above operations. Ensure that the gauze is clean.

Reconnect battery, start clock (if fitted) and test blower.

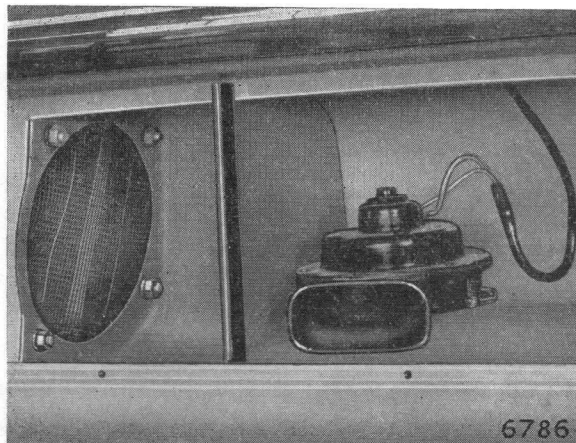


Fig. 3. Plenum chamber, blower securing nuts and filter gauze

HEATER Mk. I and II

To remove and refit (See Figs. 4 and 5)

IMPORTANT—All models: BEFORE removing the heater, disconnect windscreen washer tubes at T-piece and remove nozzles and tubes. Refit and reconnect AFTER refitting the heater.

The operations given below are for IMP. To remove the heater from the CHAMOIS model it is necessary first to remove the instrument panel on which the heater control is mounted together with the ashtray and oil pressure gauge—See Sections "O" and "N".

Disconnect battery.

Remove the luggage compartment, bulkhead trim and open bleed valve (if fitted).

Set control to "HOT" (Red) and drain cooling system (save anti-freeze if used).